











WELCOME

Thank you for visiting the Lake County Division of Transportation's open house for the **Fairfield Road Planning Study**. We appreciate you taking time to learn more about this study and provide input.

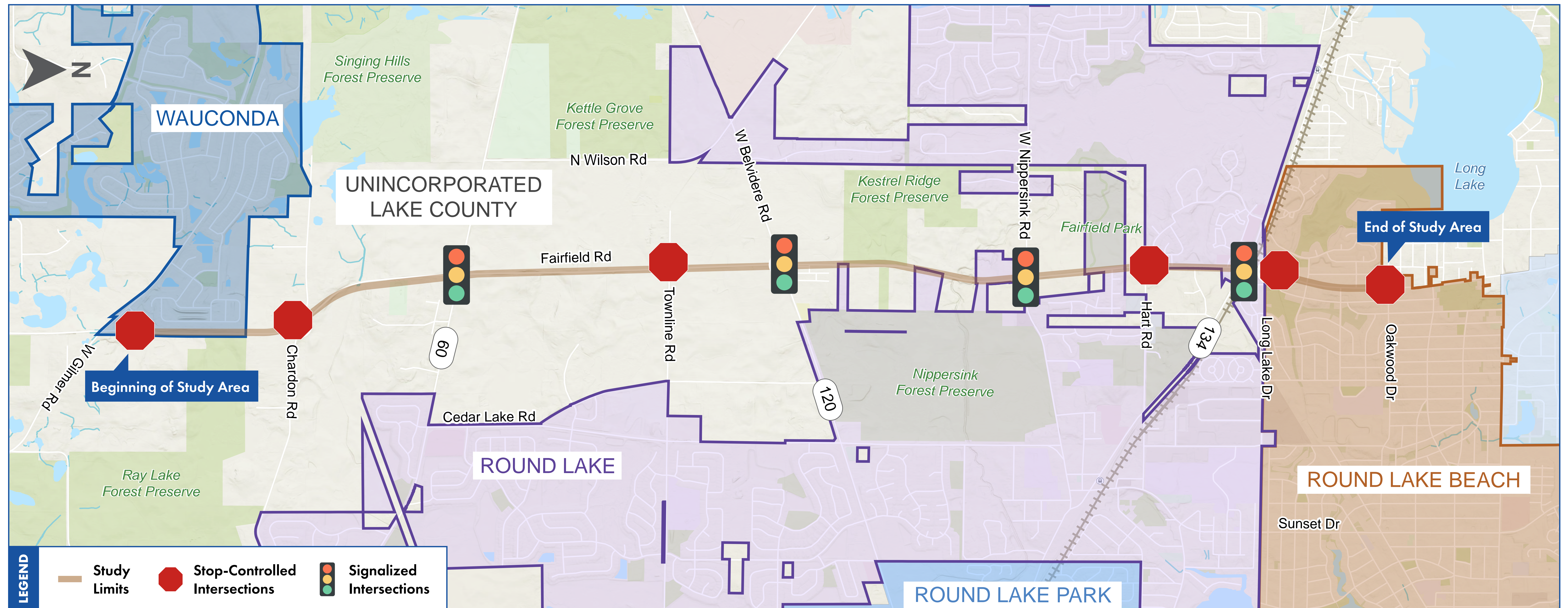
WE HOPE YOU WILL:

-  Sign-in
-  Take a Look at the Exhibits
-  Watch the Video
-  Ask Questions!
-  Provide Feedback

WHAT YOU WILL LEARN ABOUT:

-  About the Study
-  Findings of the Study
-  Next Steps

LOCATION MAP & STUDY AREA

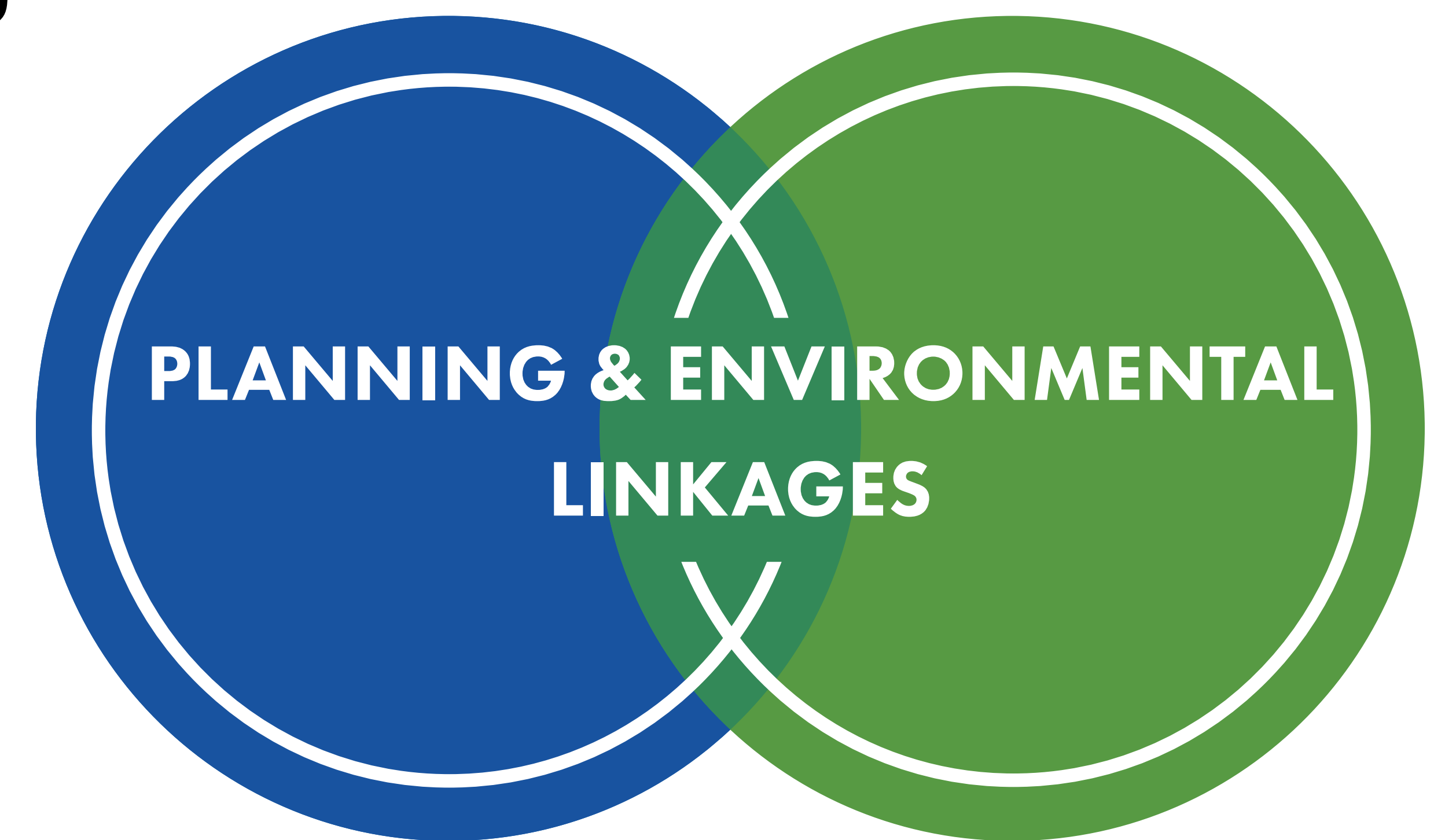


The study area follows Fairfield Road from north of Gilmer Road to Oakwood Drive, an approximately 5.5 mile segment.

WHAT IS A PEL STUDY?

A PEL or Planning and Environmental Linkages Study is a planning study that provides for:






- A collaborative approach to decision-making
- Early consideration of:
 - Environmental resources
 - Community context
 - Future development plans



WHY USE A PEL STUDY?



A PEL Study will be conducted to enable the following:

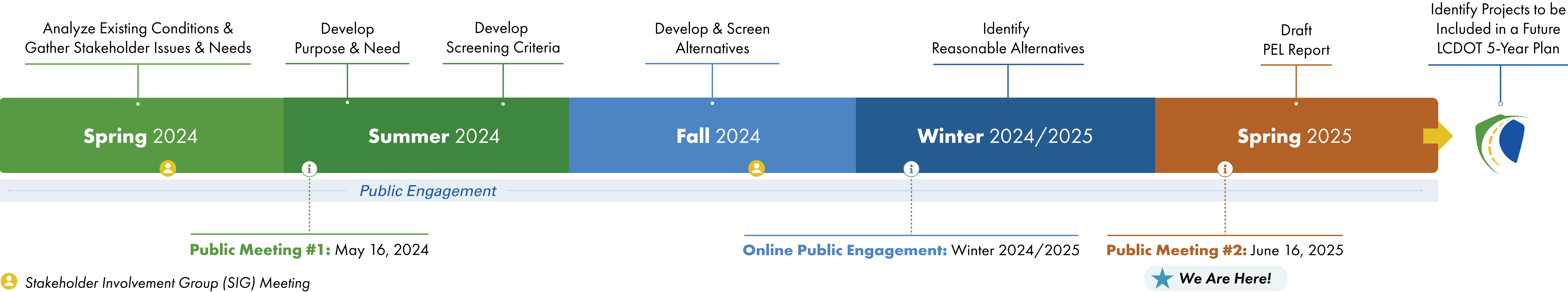
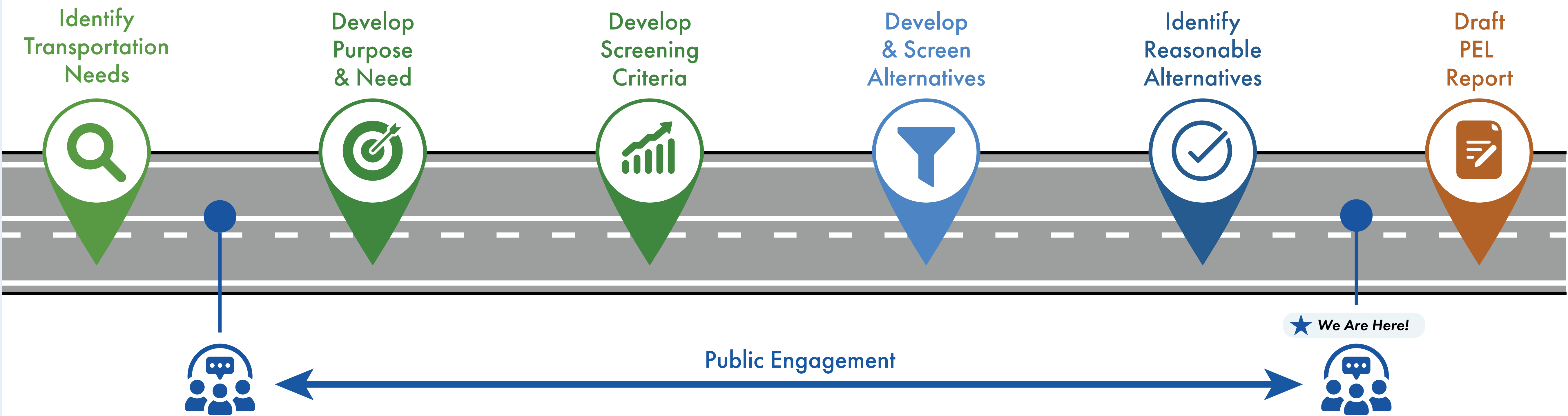
-  Draft Purpose and Need with federal and state concurrence
-  Screening of alternatives
-  Basic description of the environmental setting
-  Decision on methodologies for analysis
-  Allows project to be eligible for potential state and federal funding

The PEL will develop a roadmap for the corridor and identify potential future projects.

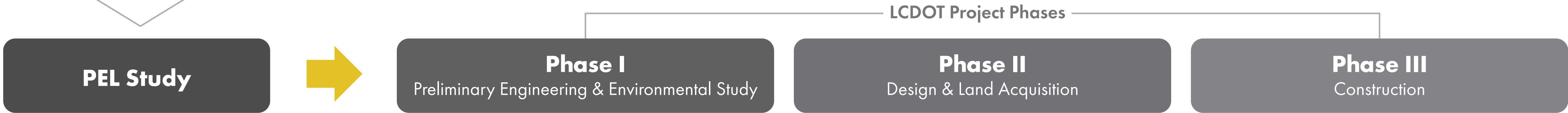
STUDY SCHEDULE AND PROCESS



WHAT ARE THE STEPS IN THE PEL PROCESS?



👤 Stakeholder Involvement Group (SIG) Meeting



-  The **Purpose** of the transportation improvements in the study area is to reduce congestion on Fairfield Road and surrounding roads and improve safety for all users.
-  The **Need** for the improvements is based on feedback from the public to enhance safety and operations, to reduce congestion, to be compatible with other planned improvements on surrounding roadways, and to consider non-motorized modes of transportation for bicyclists and pedestrians.

FEEDBACK WE'VE RECEIVED



FAIRFIELD ROAD
PLANNING STUDY

Traffic safety concerns exist at multiple intersections in the corridor, particularly at Chardon Rd., Gilmer Rd., and IL Route 60, IL Route 120, and IL Route 134.

Traffic control measures such as a 4-way-stop traffic signal or roundabout should be considered.

Excessive vehicle speed on the corridor is an issue.

Traffic volumes make traveling the corridor difficult, especially in peak hours.

Bicycle and pedestrian connectivity and safety should be a priority. Consider adding multi-use paths.

Flooding or drainage issues are an existing concern and should be prioritized in the study.

Freight traffic volume on Fairfield Road and IL 120 creates challenges.

Care should be taken to avoid or minimize environmental impacts from potential projects.

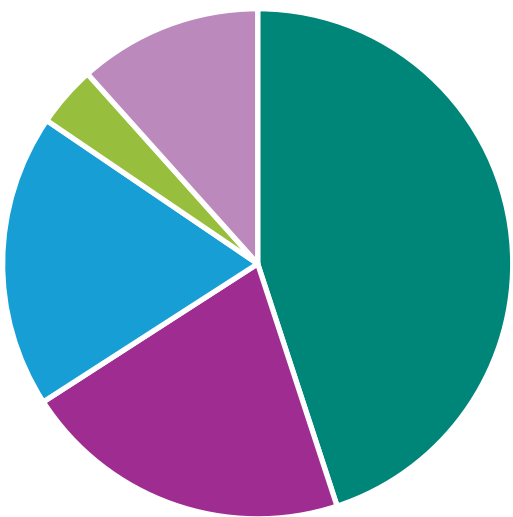
FEEDBACK BY THE NUMBERS



Online Engagement Poll Questions Responses:

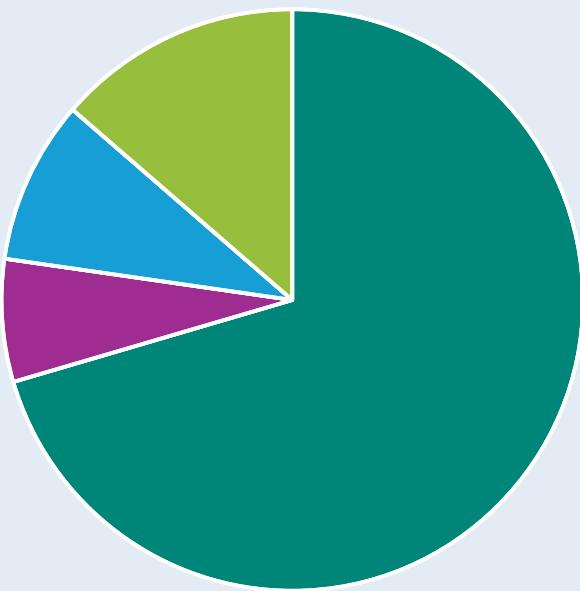


QUESTION 1
What is your bicycle/pedestrian facility preference north of IL 120?
47% Off-Road Multi-Use Path **14% Paved Shoulder**
18% On-Road Bike Lane with Curb **9% Sidewalk** **12% None**



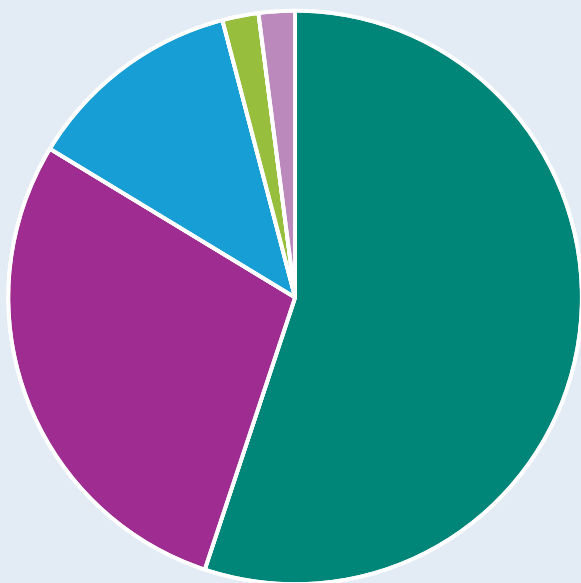
QUESTION 2
What is your bicycle/pedestrian facility preference south of IL 120?
46% Off-Road Multi-Use Path **18% Paved Shoulder**
21% On-Road Bike Lane with Curb **4% Sidewalk** **11% None**

Alternatives are evaluated for improvement to traffic operations, increased safety performance, impacts to adjacent lands, impacts to natural resources, technical feasibility, and construction cost. What additional screening criteria should be included, if any?



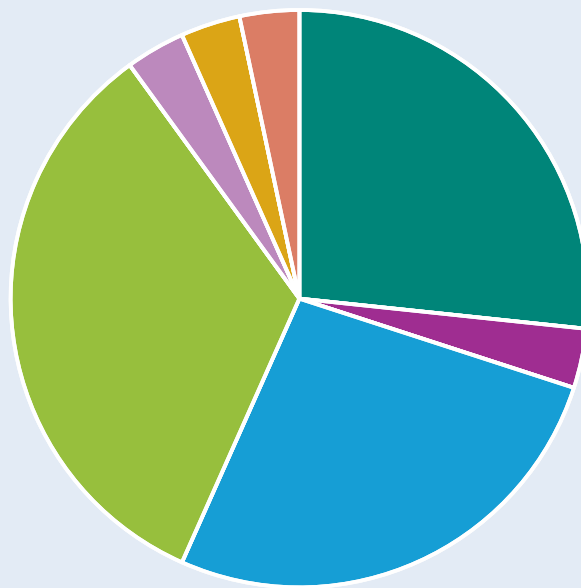
31 Bicycle/Pedestrian Safety and Connectivity
6 Traffic Operations and Congestion
4 Property Impacts and Driveway Access
3 Vehicle Safety and Driver Behavior

Online Engagement Interactive Map Comments



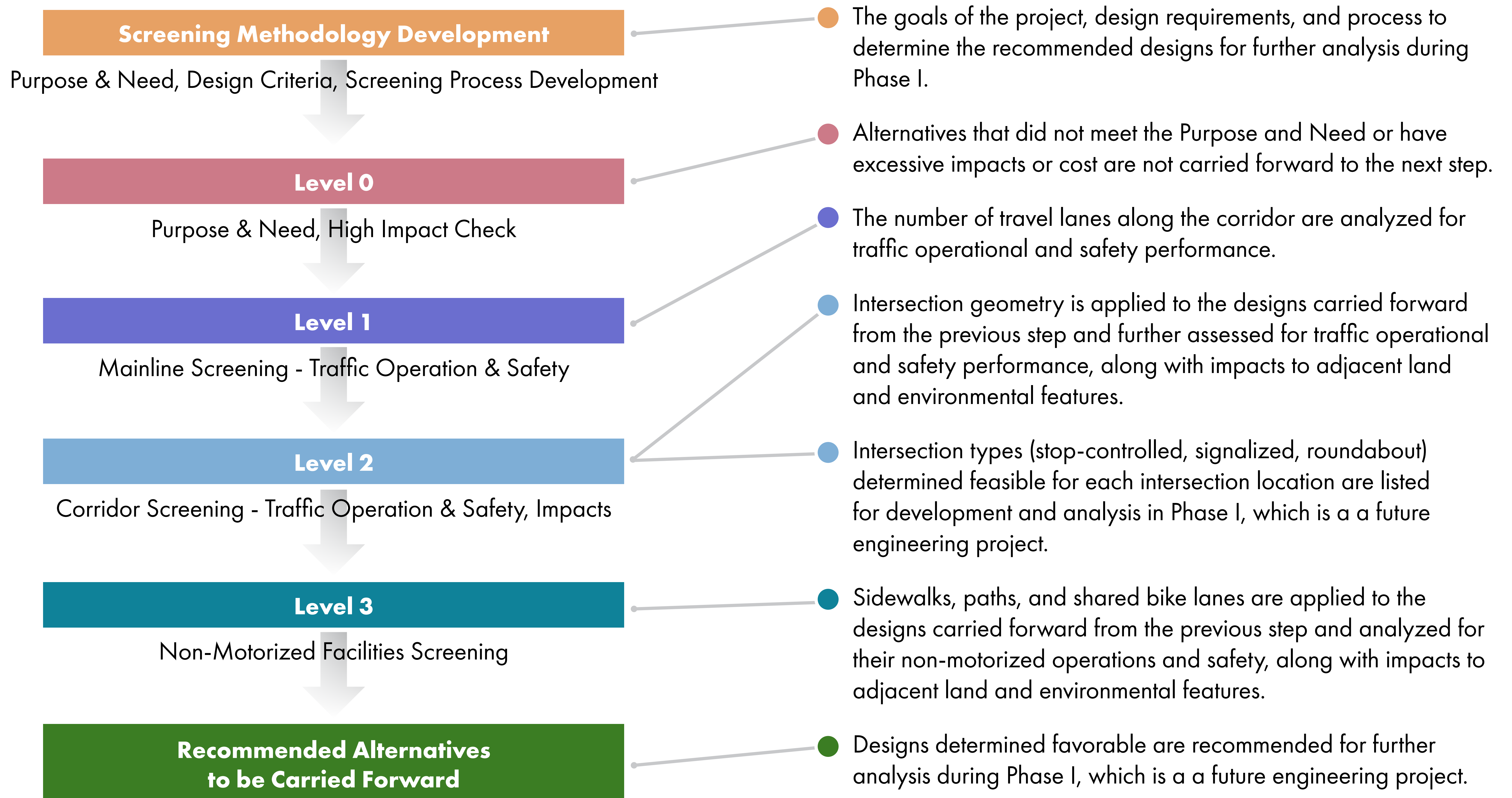
27 Bicycle/Pedestrian Connectivity
13 Safety
5 Traffic
1 Environmental
1 Other Considerations

Online Engagement Comments

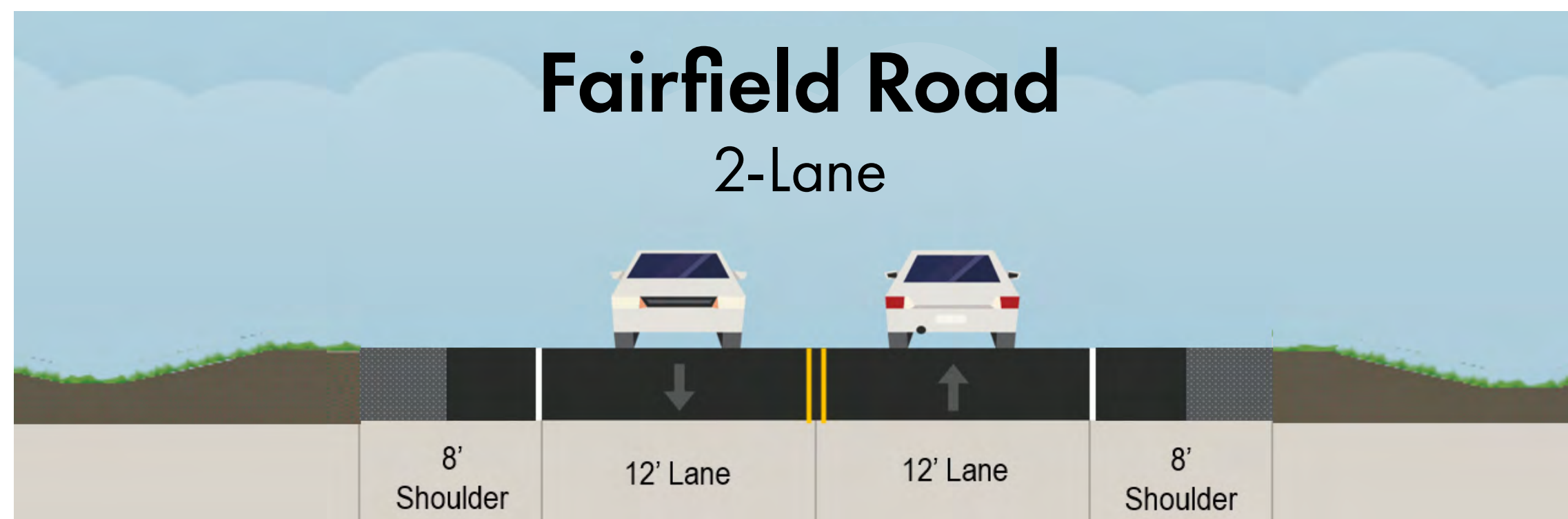


10 Improve Intersection Safety and Operations
8 Add Vehicle Capacity and/or Improve Traffic Operations
8 Bicycle/Pedestrian Safety and Connectivity
1 Avoid Adding Lanes
1 Freight Traffic Impacts
1 Limit Impacts to the Environment
1 Other

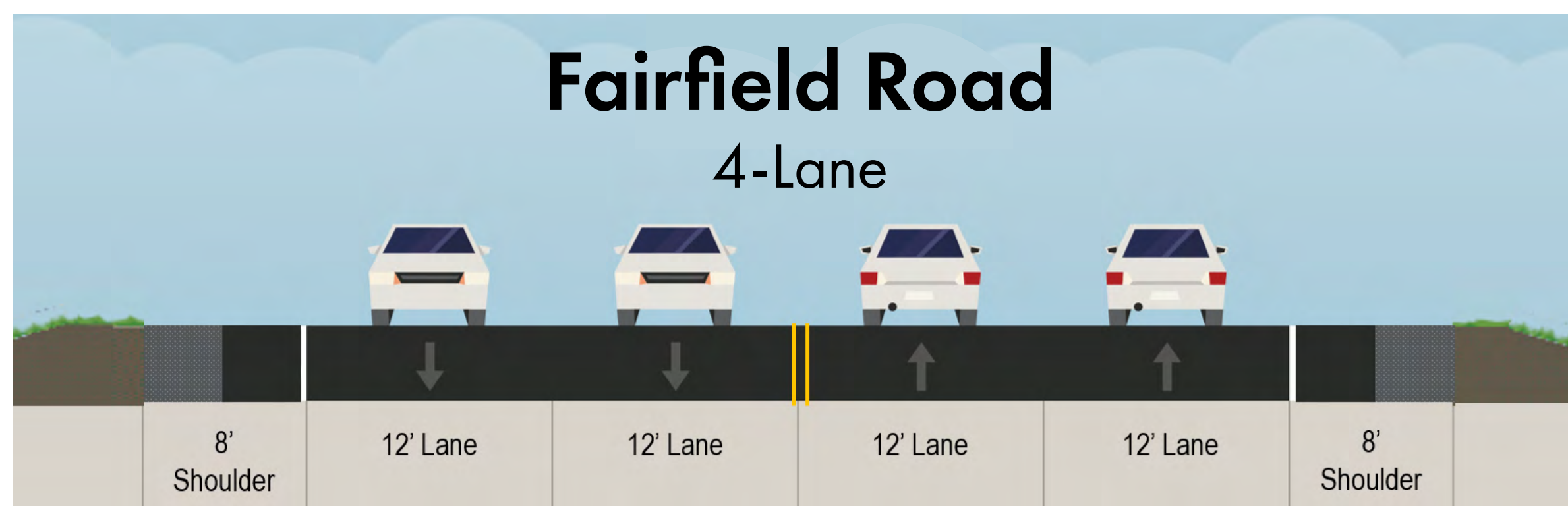
SCREENING PROCESS



ROADWAY ALTERNATIVES NOT CARRIED FORWARD

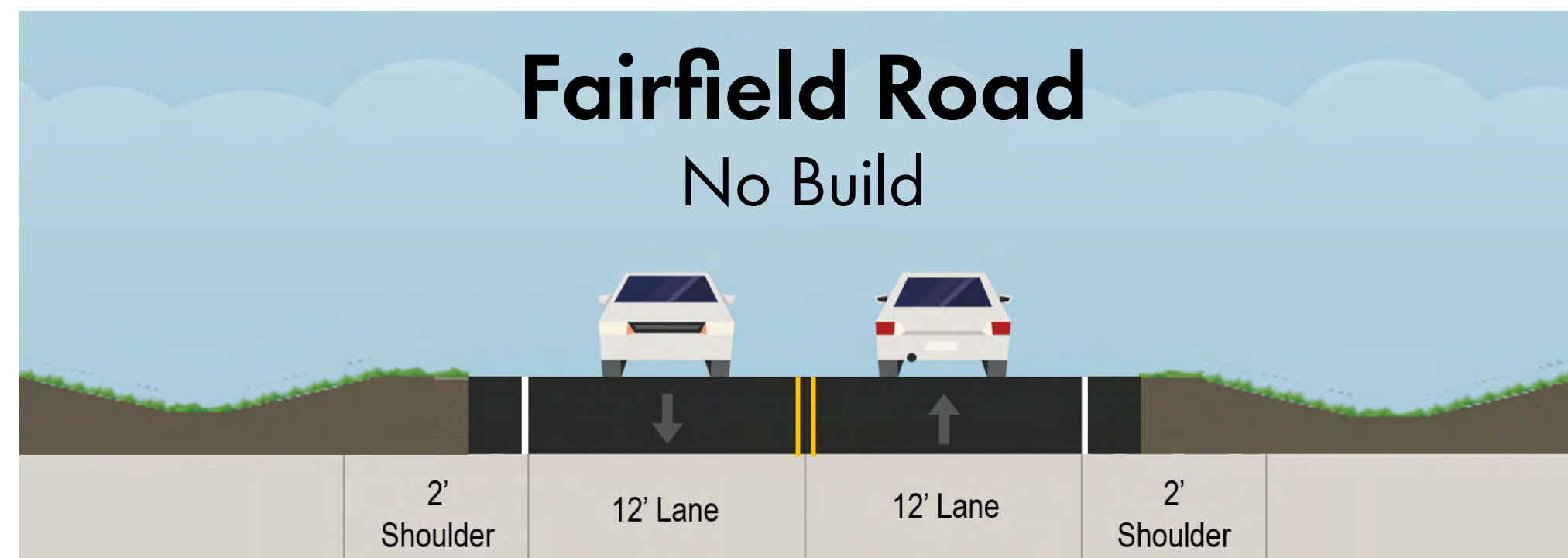


- ❌ 2-Lane**
- Does not meet Purpose and Need based on existing traffic and safety conditions



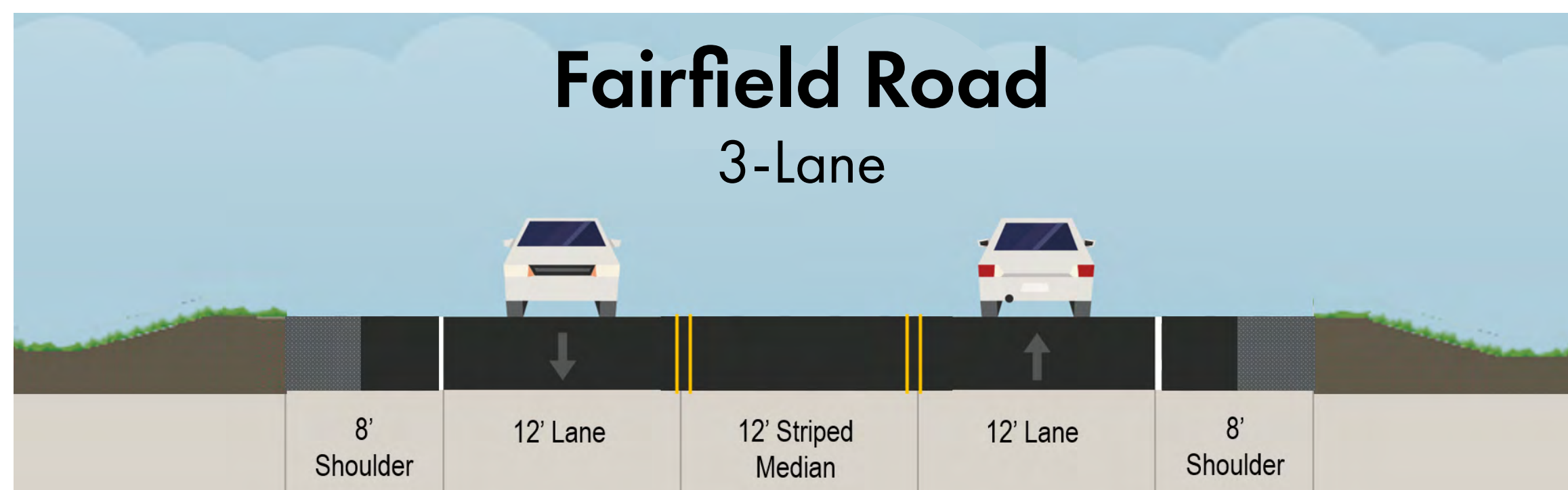
- ❌ 4-Lane**
- Moderate improvement to traffic operations
 - Low improvement to safety
 - Increased risk for rear-end crashes for vehicles making left turns as well as head-on and sideswipe crashes

ROADWAY ALTERNATIVES TO BE CARRIED FORWARD



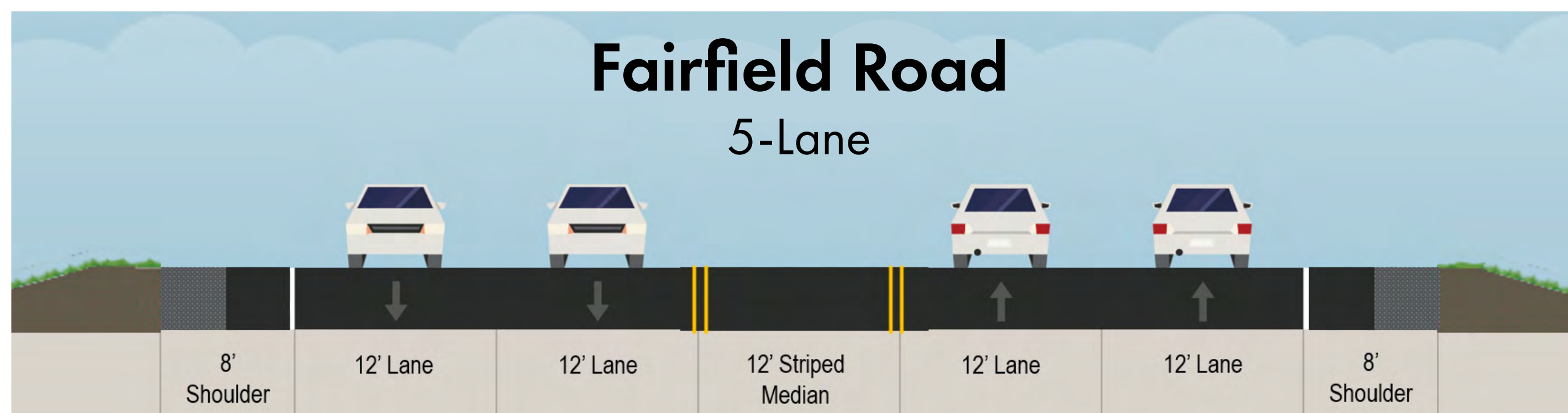
~ No Build

- Serves as a baseline comparison for other alternatives
- Federally required to advance through the PEL process



✓ 3-Lane

- Moderate improvements to traffic mobility
- Increases safety
- Moderate impacts to adjacent property and natural resources
- Improvements to bicycle and pedestrian safety via non-motorized alternatives

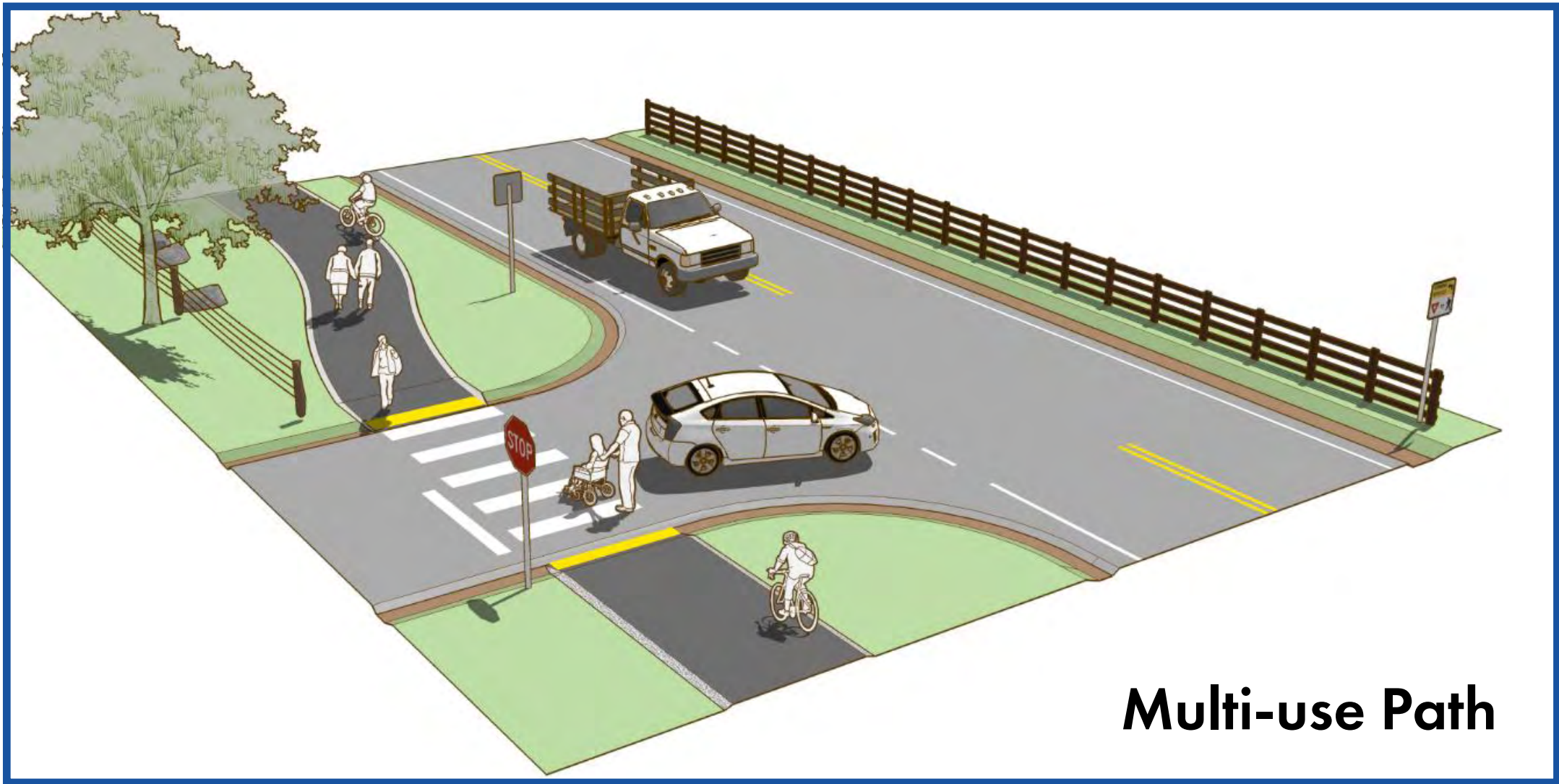
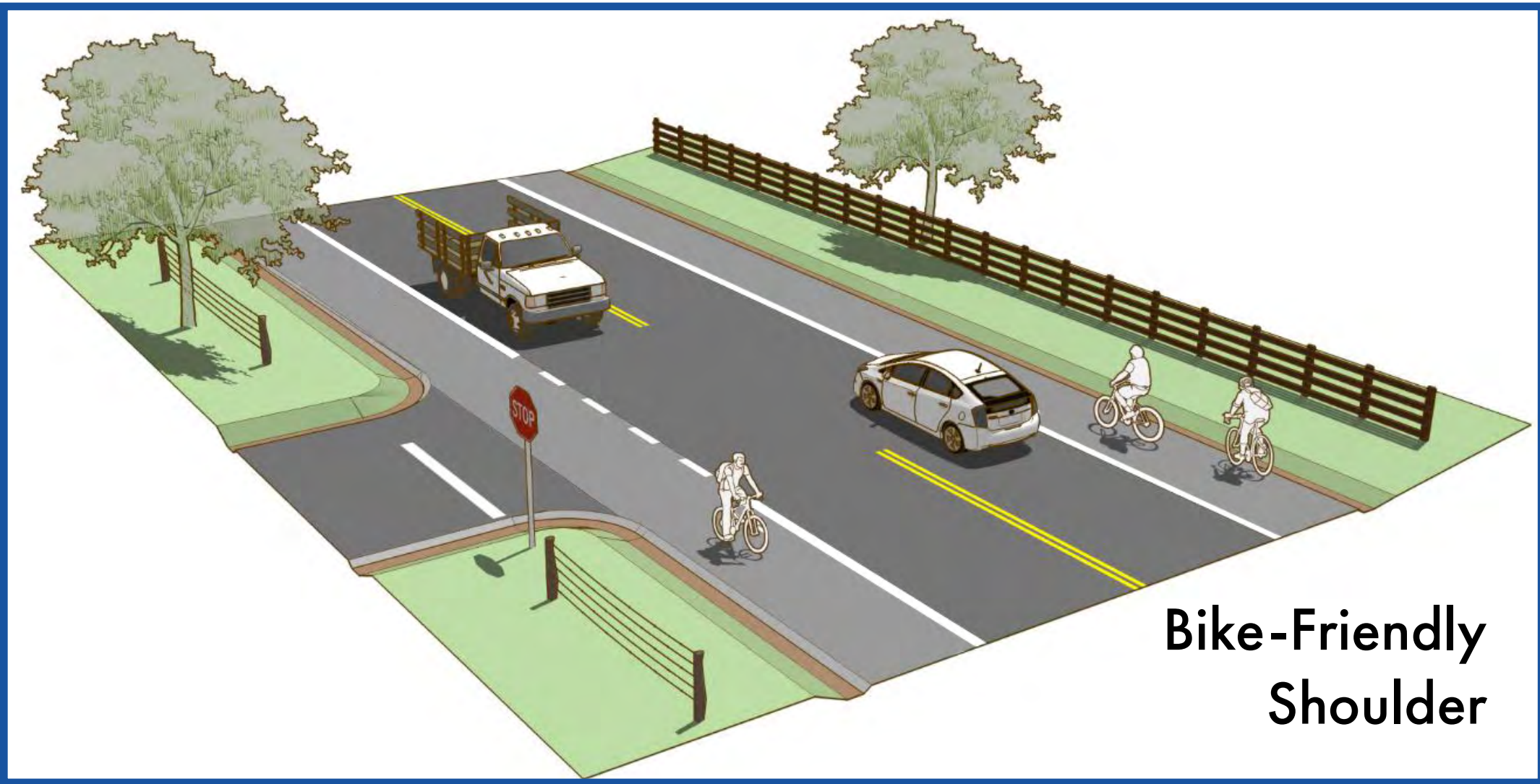
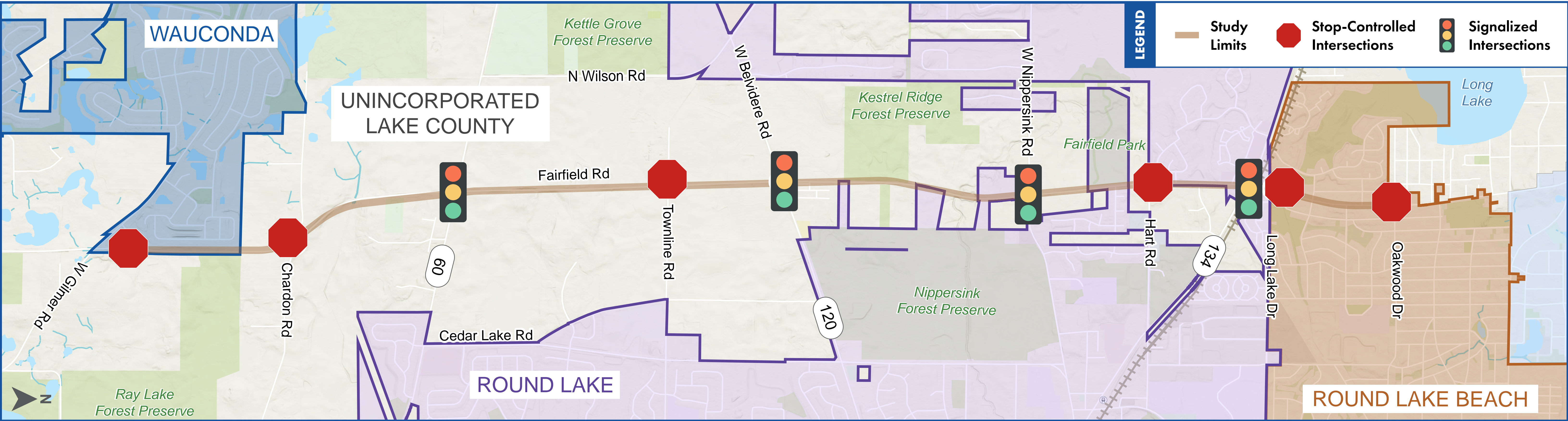


✓ 5-Lane

- High improvements to traffic mobility
- Increases safety
- Higher impacts to adjacent property and natural resources
- Improvements to bicycle and pedestrian safety via non-motorized alternatives

Non-motorized improvements like bike path and sidewalk are being considered with all alternatives. See the Non-Motorized Alternatives Board for additional information.

NON-MOTORIZED ALTERNATIVES TO BE CARRIED FORWARD



Source: Small Town and Rural Design Guide Facilities for Walking and Biking

INTERSECTION IMPROVEMENTS BEING CONSIDERED



 STOP CONTROLLED

 ROUNDABOUT

 SIGNALIZED

 RE-ALIGNMENT

INTERSECTION IMPROVEMENTS CONSIDERED					
Intersections	Additional Turn Lanes on Fairfield Road	Additional Turn Lanes on Minor Street	Improvement to Traffic Signals	Traffic Signal Installation	Roundabout
Fairfield Road at Gilmer Road				●	●
Fairfield Road at Chardon Road	●	●		●	●
Fairfield Road at IL Route 60			●		
Fairfield Road at Townline Road	●				●
Fairfield Road at IL Route 120	●	●	●		
Fairfield Road at Nippersink Road			●		●
Fairfield Road at Hart Road		●		●	●
Fairfield Road at IL Route 134	●		●		
Fairfield Road at Long Lake Drive				●	●

WHAT'S NEXT



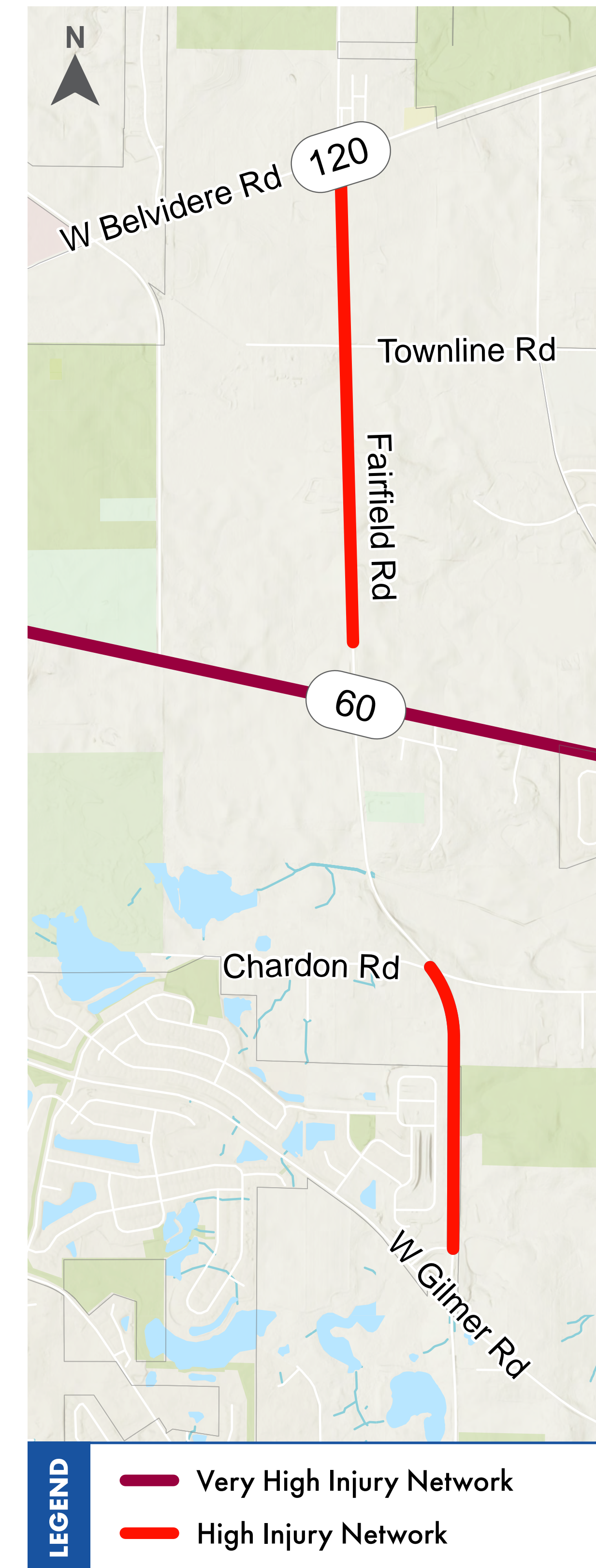
Upon completion of this meeting and the public comment period, the PEL report will be finalized and posted to the website. The report will guide programming of county funds for future engineering and construction phases of projects along the corridor. These future engineering studies will include a comprehensive analysis of the alternatives carried forward, including intersection and non-motorized alternatives, and determine the preferred alternative for each segment. Additional stakeholder and public outreach, including public meetings, will be part of these future engineering studies.

ANTICIPATED NEAR TERM IMPROVEMENTS (ANTICIPATE 1-2 YEARS)

- Finalize PEL Report (2025)
- Initiate preliminary engineering study from N. of Gilmer Road to N. of IL 60
- Resurface the roadway from IL 60 to Nippersink Road (2026)

ANTICIPATED LONGER TERM IMPROVEMENTS (ANTICIPATE 3-5 YEARS)

- Initiate preliminary engineering study (N. of IL 60 to N. of IL 120)
- Initiate preliminary engineering study (N. of IL 120 to N. of IL 134)



YOUR INPUT



The comment period is open **JUNE 16 – JULY 14, 2025**



Fill Out a Comment Form



Connect@FairfieldRoadStudy.com



FairfieldRoadStudy.com



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Scan the
QR code
to provide
additional
feedback!

